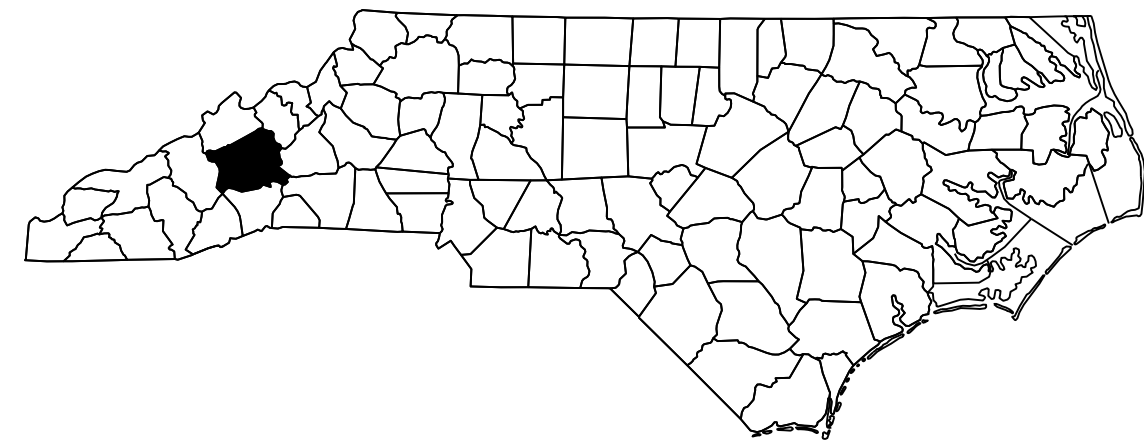


PROJECT: 41665.13B

CONTRACT: DM00300



STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

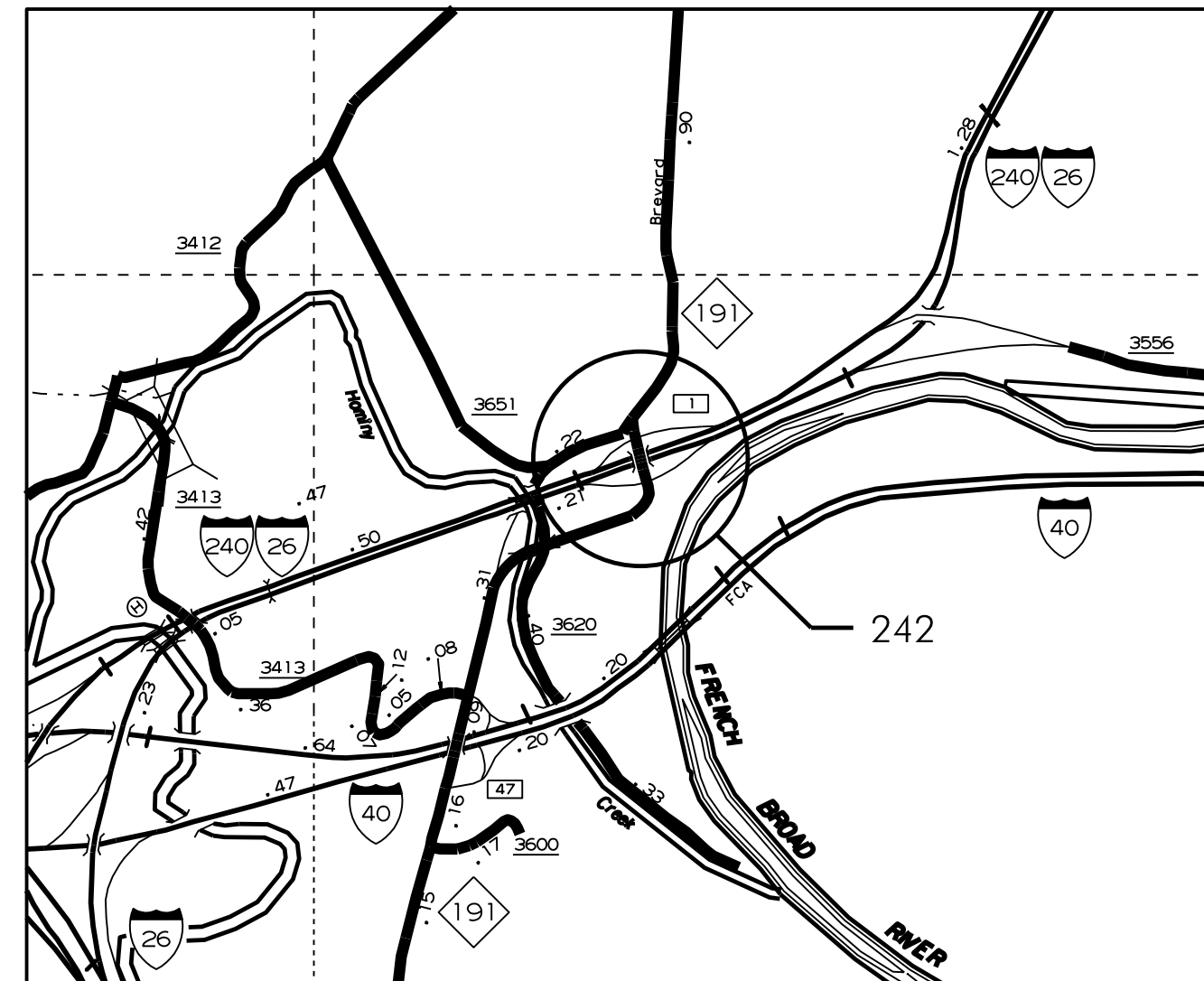
BUNCOMBE COUNTY

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	41665.13B	1	
STATE PROJ. NO.	P.A. PROJ. NO.	DESCRIPTION	
41665.13B	-	P.E.	
41665.13B	-	CONST.	

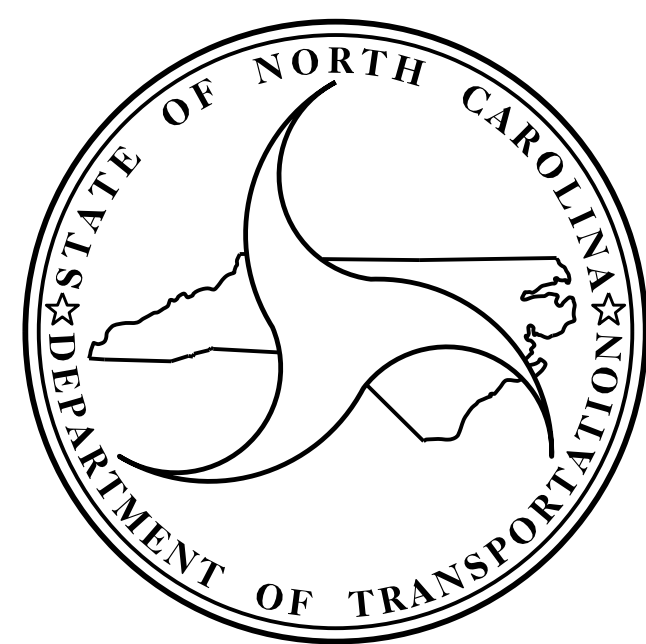
LOCATION: BUNCOMBE COUNTY

BRIDGE #100242 ON NC 191 (BREVARD RD.) OVER INTERSTATE 240 /26

TYPE OF WORK: BEAM REPLACEMENT



VICINITY MAP - BUNCOMBE CO.



DESIGN DATA

BUNCOMBE COUNTY
#242 ADT 2016 = 9300

PROJECT LENGTH

BUNCOMBE COUNTY
- #242 = 0.028 MILE

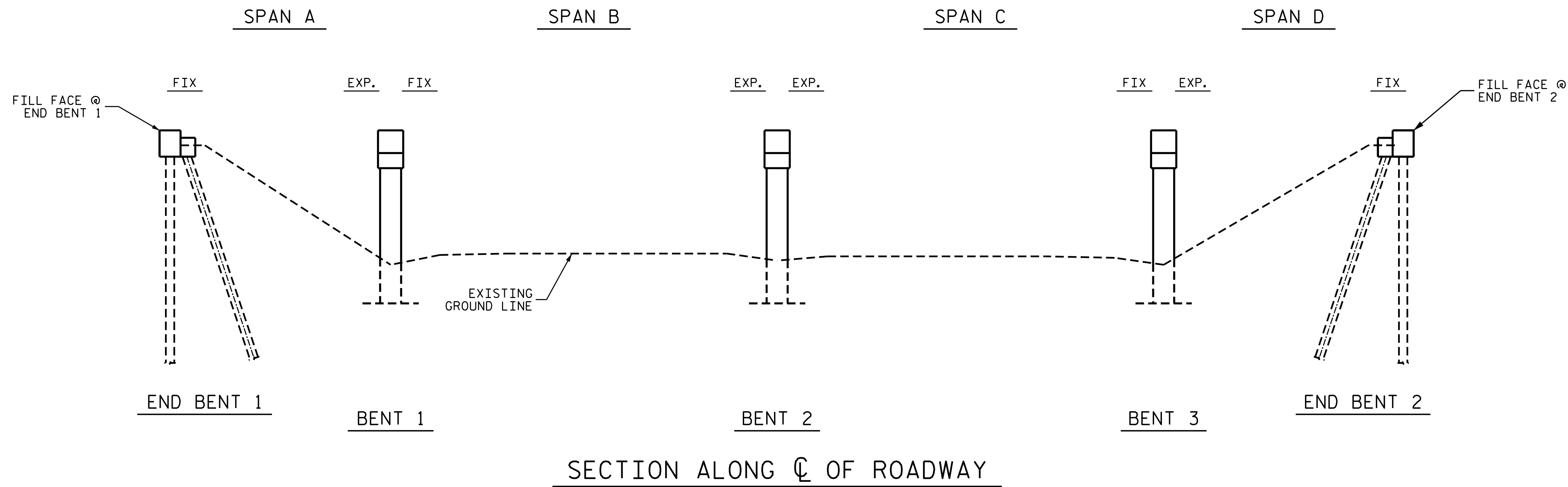
Prepared In the Office of:
DIVISION OF HIGHWAYS
STRUCTURES MANAGEMENT UNIT
1000 BIRCH RIDGE DR.
RALEIGH, N.C. 27610

2018 STANDARD SPECIFICATIONS

LETTING DATE : DECEMBER 2, 2020

A. KEITH PASCHAL, P.E.
PROJECT ENGINEER

AMBER M. LEE, P.E.
PROJECT DESIGN ENGINEER



NOTES

PROFILE INFORMATION IS TAKEN FROM THE ORIGINAL PLANS AND THE ROUTINE INSPECTION REPORT DATED 2/08/2020.

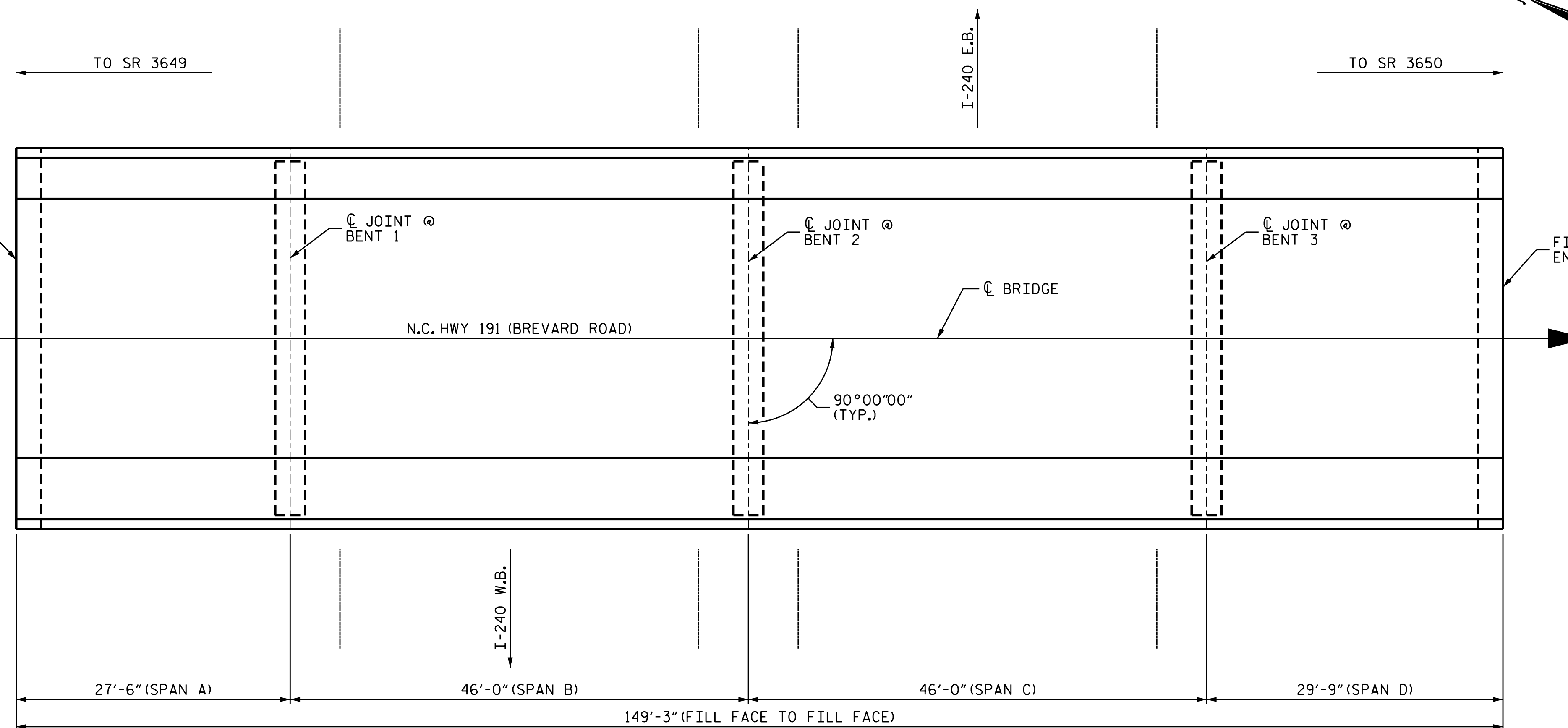
BRIDGE ORIENTATION CONFORMS TO EXISTING BRIDGE PLANS.

SCOPE OF WORK

- BEAM 5 IN SPAN B REPLACEMENT.
- BEARING REPLACEMENT FOR BEAM 5 IN SPAN B.
- BENT DIAPHRAGM AND INTERMEDIATE DIAPHRAGM FOR BEAM 5 REPLACEMENT.
- CLEANING AND PAINTING OF NEW STEEL AND DAMAGED PAINT AREAS.

CONSTRUCTION SEQUENCE

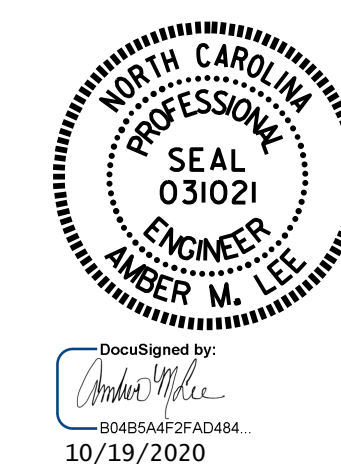
- 1.) FIELD SURVEY AND MEASURE EXISTING BEARING HEIGHTS AND VERIFY PROPOSED BEAM CUT OUT DIMENSIONS FOR PREPARATION OF SHOP PLANS.
- 2.) PERFORM BEAM OR SPAN JACKING AS REQUIRED. PERFORM NECESSARY CONCRETE REPAIRS TO DELAMINATED OR OTHERWISE DETERIORATED AREAS OF CAP UNDER OR ADJACENT TO BEARINGS.
- 3.) REMOVE EXISTING NON-COMPOSITE W33X130 BEAM 5 AND REPLACE WITH W30X132 BEAM WITH STUB COLUMN BEARINGS. (REQUIRES ROAD CLOSURE-SEE TRANSPORTATION MANAGEMENT PLANS)
- 4.) REMOVE EXISTING C15X33.9 DIAPHRAGMS AND REPLACE WITH CHANNEL C15X33.9 FOR BENTS AND INTERMEDIATE DIAPHRAGMS.
- 5.) CLEAN TOPS OF BENT 1 & 2 CAPS AT REPLACEMENT OF BEAM 5. PERFORM SURFACE PREPARATION, AND APPLY EPOXY CAP COATING AROUND BEAM 5 REPLACEMENT AREA.



PLAN

I HEREBY CERTIFY THAT THIS STRUCTURE WAS REHABILITATED ACCORDING TO THESE PLANS OR AS NOTED HEREIN.

RESIDENT ENGINEER _____ DATE _____



PROJECT NO. 41665.13B
BUNCOMBE COUNTY
 BRIDGE NO. 100242
 SHEET 1 OF 2

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 GENERAL DRAWING
 FOR BRIDGE ON NC 191
 (BREVARD ROAD)
 OVER INTERSTATE 240

DRAWN BY : H.A. LOCKLEAR DATE : 05/2020
 CHECKED BY : A.M. LEE, PE DATE : 7/2020

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	TOTAL SHEETS
1			3			S-01
2			4			6



LOCATION SKETCH

INFORMATION INDICATED ON THE LOCATION SKETCH SHALL BE CONSIDERED GENERAL INFORMATION, ONLY. THE CONTRACTOR SHALL CONFIRM, THROUGH OTHER SOURCES, SPECIFIC INFORMATION REGARDING BRIDGES, ROADWAYS, UTILITIES, THE SURROUNDING AREA, AND ANY OTHER ASPECTS THAT MAY BE NECESSARY TO PERFORM AND COMPLETE THE PROJECT.

BRIDGE COORDINATES

LATITUDE: 35.56390278
LONGITUDE: 82.59388333

TOTAL BILL OF MATERIAL					
BRIDGE No.	APPROX. 7,058 LBS STRUCTURAL STEEL	PARTIAL REMOVAL OF EXISTING STRUCTURE NO. ---	UNDER STRUCTURE PLATFORM	TYPE I BRIDGE JACKING NO.---	ELASTOMERIC BEARING, MODIFIED TYPE I-B
	LUMP SUM	LUMP SUM	LUMP SUM	EA.	EA.
242	LUMP SUM	LUMP SUM	LUMP SUM	2	2

NOTE

AT THE TIME OF PREPARATION OF THESE PLANS, IT WAS NOT ANTICIPATED THAT CONCRETE REPAIRS AND SHOTCRETE REPAIRS WOULD BE REQUIRED. HOWEVER, IT MAY BE DETERMINED IN THE FIELD THAT CONCRETE REPAIRS AND SHOTCRETE REPAIRS, OR OTHER WORK WILL BE NECESSARY TO PROPERLY COMPLETE THE INTENDED BRIDGE REHABILITATION WORK. THE CONTRACTOR SHALL BE PREPARED TO PERFORM SUCH WORK IN A TIMELY MANNER, AS DETERMINED IN THE FIELD. SUCH WORK SHALL BE CONSIDERED EXTRA WORK AND SHALL BE ADDRESSED AS PER ARTICLE 104-7 OF THE STANDARD SPECIFICATIONS. PROJECT SPECIAL PROVISIONS THAT OUTLINE REQUIREMENTS FOR THESE POTENTIAL ADDITIONAL WORK ITEMS HAVE BEEN PROVIDED IN PROJECT DOCUMENTS, BUT NO QUANTITIES HAVE BEEN LISTED. ACTUAL PAY ITEMS, QUANTITIES, AND COSTS WILL BE ESTABLISHED, AS REQUIRED, IF EXTRA WORK IS ENCOUNTERED.

UNANTICIPATED ITEMS:

ITEM NO.	DESCRIPTION	UNIT
1	CONCRETE REPAIR	CU. FT.
2	SHOTCRETE REPAIR	CU. FT.

NOTES

EXISTING DIMENSIONS AND BRIDGE CONDITION ARE FROM THE BEST INFORMATION AVAILABLE. THE CONTRACTOR SHALL FIELD VERIFY THE INFORMATION SHOWN ON THE PLANS AND NOTIFY THE ENGINEER IF ACTUAL DIMENSIONS AND CONDITIONS DIFFER.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL STATE AND FEDERAL SAFETY REQUIREMENTS.

FOR CONTROL OF TRAFFIC AND LIMITS ON PHASING OF CONSTRUCTION, SEE TRANSPORTATION MANAGEMENT PLANS.

FOR MAINTENANCE AND PROTECTION OF TRAFFIC BENEATH BRIDGE, SEE SPECIAL PROVISIONS.

FOR PARTIAL REMOVAL OF EXISTING STRUCTURE NO. 242, SEE SPECIAL PROVISIONS.

FOR MODIFIED ELASTOMERIC BEARINGS, SEE SPECIAL PROVISIONS.

FOR SHOTCRETE REPAIRS, SEE SPECIAL PROVISIONS.

FOR CONCRETE REPAIRS, SEE SPECIAL PROVISIONS.

FOR BRIDGE JACKING, SEE SPECIAL PROVISIONS.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR UNDER STRUCTURE PLATFORM, SEE SPECIAL PROVISIONS.

PROJECT NO. 41665.13B
BUNCOMBE COUNTY
BRIDGE NO. 100242

SHEET 2 OF 2



DocuSigned by:
Amber W. Lee
80658A0-25AD484
10/19/2020

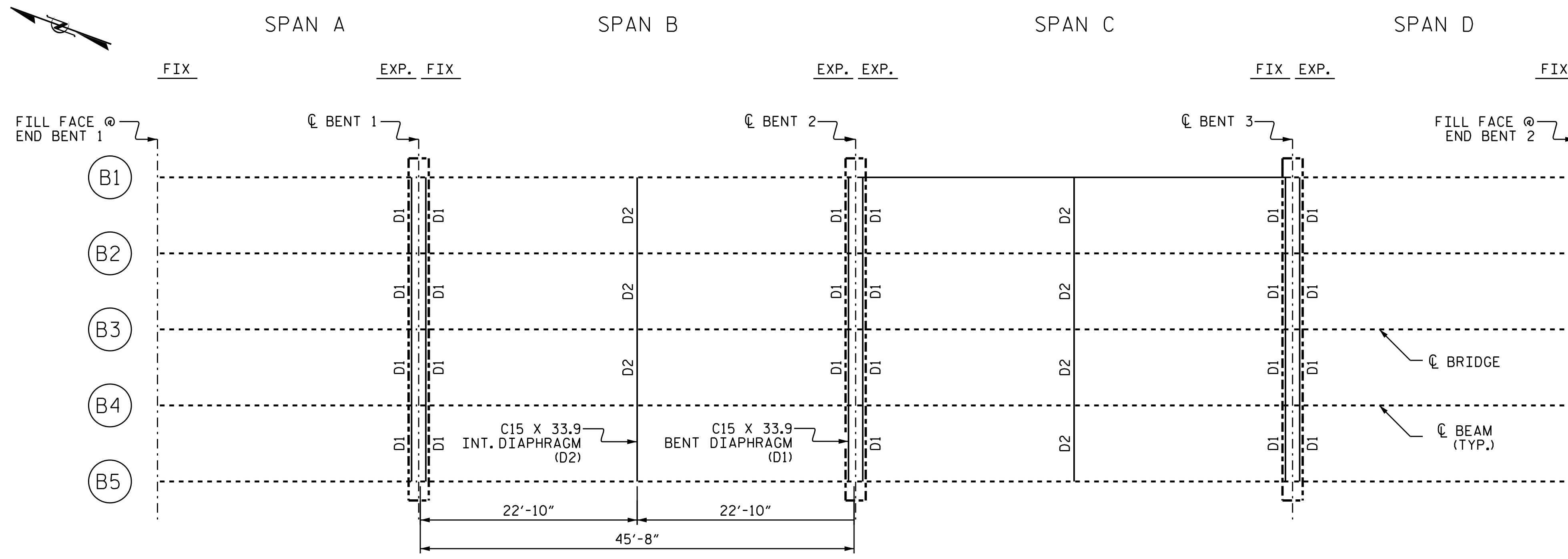
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

GENERAL DRAWING
FOR BRIDGE ON NC 191
(BREVARD ROAD)
OVER INTERSTATE 240

DRAWN BY : H.A. LOCKLEAR DATE : 05/2020
CHECKED BY : A.M. LEE DATE : 08/2020

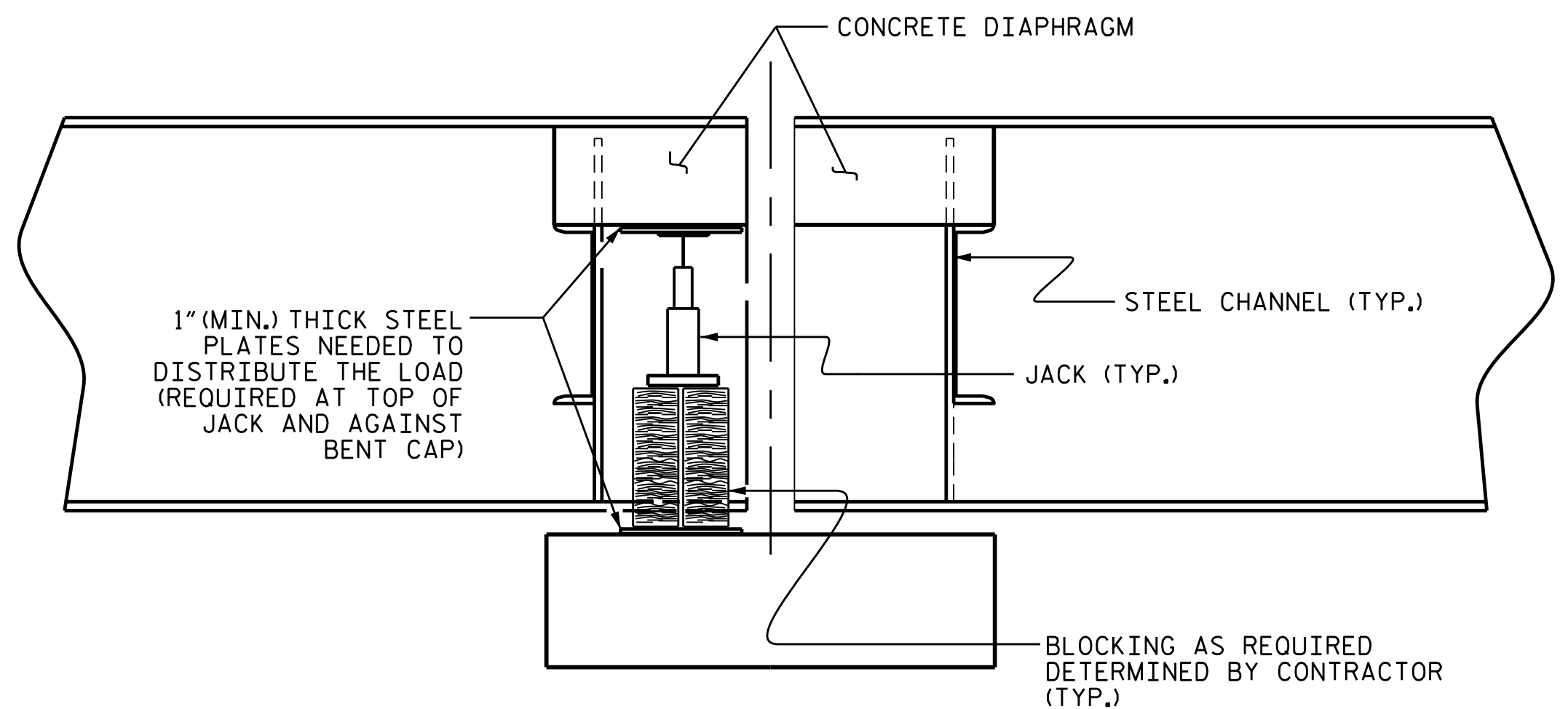
DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-02
1			3			TOTAL SHEETS
2			4			6



NOTE: EXISTING BEAM 5 (W33X130) IN SPAN B TO BE REPLACED WITH W30X132X45'-8".

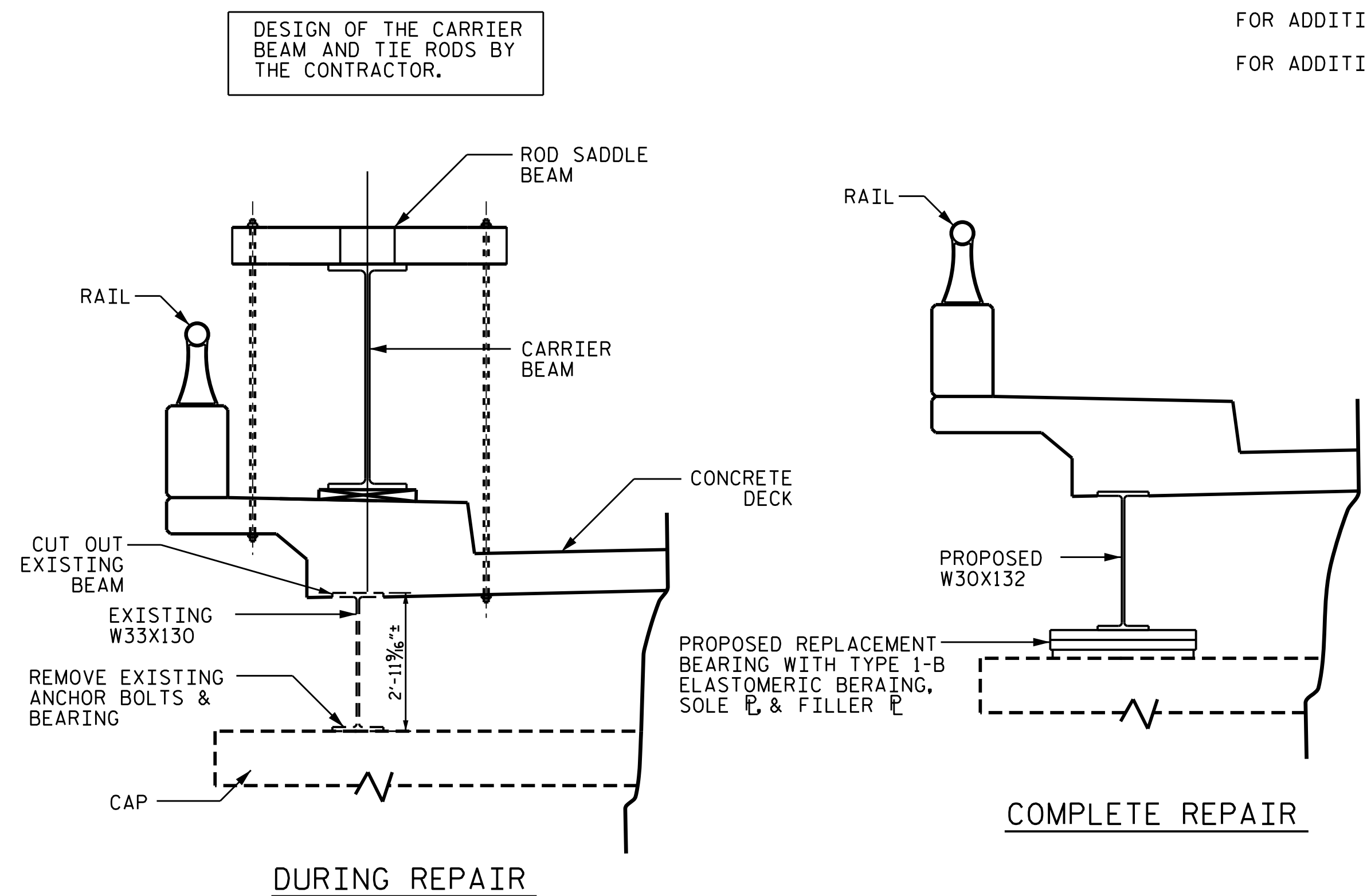
FRAMING PLAN



SECTION THRU DIAPHRAGM

DRAWING PROVIDED AS AN EXAMPLE OF A TYPICAL BRIDGE JACKING SET-UP AND IS FOR INFORMATION PURPOSES ONLY. CONTRACTOR SHALL DESIGN AND SUBMIT APPROPRIATE SET-UP FOR SPECIFIC BRIDGE JACKING.

BRIDGE JACKING TABLE					
STRUCTURE NUMBER	LOCATION	SPAN	BEAM(S)	BRIDGE JACKING TYPE	DEAD LOAD (DC+DW) (KIPS)
100242	BENT 1	B	5	TYPE I	34
100242	BENT 2	B	5	TYPE I	34



DURING REPAIR

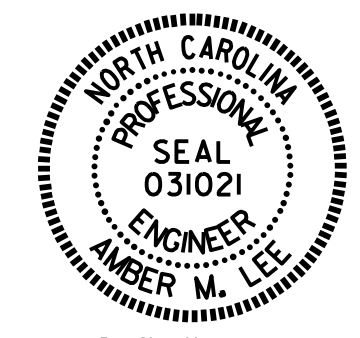
COMPLETE REPAIR

NOTES:

- CONTRACTOR SHALL SUBMIT JACKING PLANS AND CALCULATIONS SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF NORTH CAROLINA FOR REVIEW AND APPROVAL.
- THE CONTRACTOR SHALL PROVIDE BLOCKING FOR ALL JACKS AS NECESSARY. A BLOCKING PLAN SHALL BE INCLUDED AS PART OF THE JACKING PLANS.
- PRIOR TO BRIDGE JACKING, THE CONTRACTOR SHALL ENSURE THERE ARE NO OBSTACLES PREVENTING THE SPAN FROM BEING LIFTED. THIS MAY INCLUDE BUT NOT LIMITED TO METAL RAILINGS AND UTILITIES.
- THE CONTRACTOR MAY NEED TO REINFORCE EXISTING BRIDGE MEMBERS OR ADD MEMBERS TO WITHSTAND THE JACKING FORCES.
- PROVISIONS SHALL BE MADE TO ACCOUNT FOR THERMAL MOVEMENTS OR LATERAL FORCES SUCH AS WIND LOADS DURING THE PERIOD THAT THE STRUCTURE IS RESTING ON THE TEMPORARY SUPPORTS.
- ALL JACKS AND JACKING SUPPORTS SHALL BE PLUMB.
- EACH HYDRAULIC JACK SHALL HAVE A RATED CAPACITY CLEARLY SHOWN, WITH MINIMUM RATED CAPACITY OF 1.3 TIMES THE CALCULATED LOAD REACTION ADJACENT TO THE POINT OF JACKING.
- JACKS WITHOUT A MECHANICAL LOAD HOLDER (LOCK-OFF) SHALL BE SECURED BY BLOCKING IF THE JACKING OPERATION IN ANY ONE LOCATION LASTS LONGER THAN 30 MINUTES.
- HYDRAULIC SYSTEM SHALL BE CONNECTED SUCH THAT ALL JACKS LIFT SIMULTANEOUSLY.
- LIFTING FRAME SHALL EXTEND BEYOND THE LENGTH OF THE LIFTED SPAN AND PROVIDE BEARINGS AT THE SAME LOCATION AS THE ADJACENT GIRDER BEARINGS.
- CONTRACTOR SHALL SHIM BRIDGE SPAN DURING JACKING SUCH THAT THE MAXIMUM UNSHIMMED LIFT IS 1".
- CONTRACTOR SHALL PROVIDE SPAN LIFT POINTS AS CLOSE AS POSSIBLE TO THE FACE OF BENT CAP.
- IF DURING THE JACKING PROCESS OR WHILE THE SPAN IS BEING SUPPORTED, THE BEAMS SHIFT FROM THEIR ORIGINAL POSITION, ALL WORK SHALL CEASE AND THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY.
- ALL ADJACENT BEARINGS OF BEAMS NOT BEING JACKED MAY BE LOOSENED TO DECREASE THE RESISTANCE OF THE DECK SLAB DURING JACKING. ALL BEARING LOOSENED SHALL BE TIGHTENED BACK AFTER THE BEAMS ARE REPAIRED AND THE JACKS AND BLOCKING HAVE BEEN REMOVED.
- PRIOR TO INSTALLING BEARING PEDESTALS AND NEW BEARINGS, CONTRACTOR SHALL MAKE ANY REPAIRS TO BENTS AS REQUIRED IN THE CONTRACT DOCUMENTS.
- TRAFFIC SHALL NOT BE ALLOWED ON THE STRUCTURE UNTIL THE WORK REQUIRED BY THE CONTRACT DOCUMENTS IS COMPLETE.
- FOR ADDITIONAL INFORMATION ON "BRIDGE JACKING", SEE SPECIAL PROVISIONS.
- FOR ADDITIONAL NOTES, SEE "REPLACEMENT BEAM AND DIAPHRAGMS" SHEET.

PROJECT NO. 41665.13B
BUNCOMBE COUNTY
 BRIDGE NO. 100242

SHEET 1 OF 4



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**DECK UNDERSIDE REPAIR
 SPAN B**

DRAWN BY : H.A. LOCKLEAR DATE : 05/2020
 CHECKED BY : A.M. LEE DATE : 08/2020

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-03
1			3			TOTAL SHEETS
2			4			6

NOTES:

ALL MATERIALS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES.

ALL NEW STEEL ASSOCIATED WITH REPLACEMENT OF THE BEAM SHALL BE PREPARED AND SHOP PRIMED WITH ORGANIC ZINC RICH PRIMER ACCORDING TO PAINT SYSTEM #6 OF THE NCDOT STRUCTURAL STEEL COATING PROGRAM.

UNLESS NOTED OTHERWISE, ALL STEEL ON THIS DRAWING SHALL MEET THE REQUIREMENTS OF AASHTO M270 (GRADE 50) AND ITS SUPPLEMENTARY LONGITUDINAL CHARPY V-NOTCH TEST REQUIREMENTS (FOR AASHTO M270 ZONE 1). ASTM A-572 (GR 50) OR A-588 (GR 50) STEEL MAY BE SUBSTITUTED AS LONG AS THE SUPPLEMENTARY REQUIREMENTS TO THE ABOVE AASHTO SPECS ARE MET.

WEB STIFFENERS AND CONNECTOR PLATES AS NECESSARY TO MATCH EXISTING.

USING HAND OR POWER TOOL CLEANING, REMOVE PAINT OR ANY OTHER COATING AT THE LOCATION OF FIELD WELDS PRIOR TO WELDING. PROVIDE CONTAINMENT, POLLUTION CONTROL, AND DISPOSAL OF PAINT AND DEBRIS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS WHEN REMOVING PAINT OR PRIMER AND WHEN PREPARING SURFACES AND APPLYING PAINT.

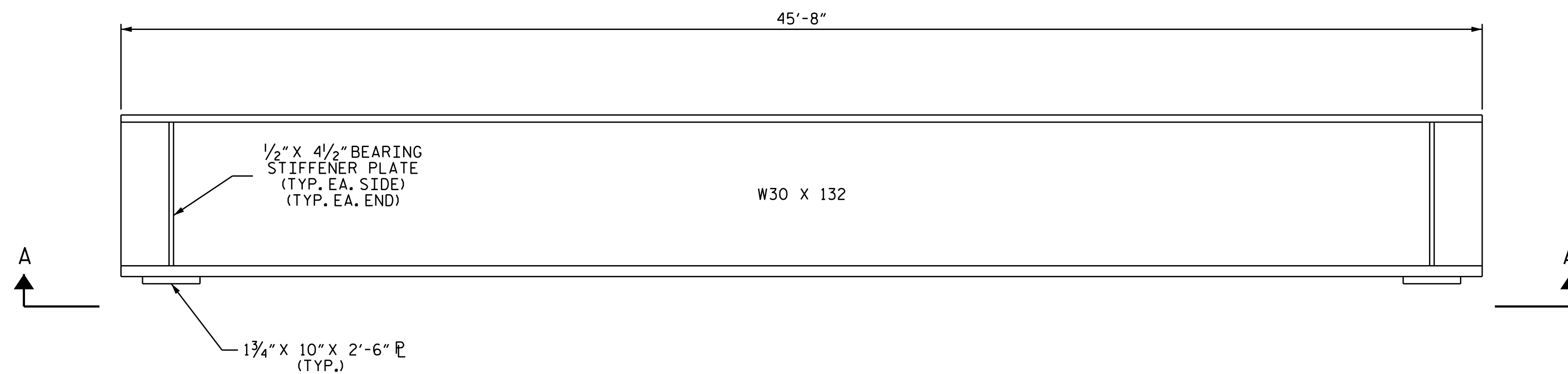
AFTER COMPLETION OF FIELD WELDING, THE WELDS AND AREAS WHERE PAINT HAS BEEN REMOVED OR DAMAGED SHALL BE REPAIRED AS PER ARTICLE 442-12 OF THE STANDARD SPECIFICATION. NEW STEEL SHALL RECEIVE A TOP COAT OF HIGH BUILD ACRYLIC IN ACCORDANCE WITH PAINT SYSTEM #6. TOPCOAT SHALL BE APPLIED IN FIELD WITH BRUSHES OR ROLLERS. APPROPRIATELY CLEAN AND PREPARE SURFACES, PRIOR TO PLACEMENT OF TOP COAT, IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. WHERE PAINT WILL BE APPLIED OVER EXISTING PAINT SYSTEM, THE NEW PAINT SHALL OVERLAP THE EXISTING BY A MINIMUM OF 6". ALL PAINT OPERATIONS, INCLUDING, BUT NOT LIMITED TO, CONTAINMENT, POLLUTION CONTROL, CLEANING, PREPARATION, REPAIR AND APPLICATION, SHALL BE CONSIDERED INCIDENTAL TO THE OTHER PROJECT PAY ITEMS.

THE CONTRACTOR SHALL VERIFY THE BOLT SPACING PRIOR TO FABRICATION.

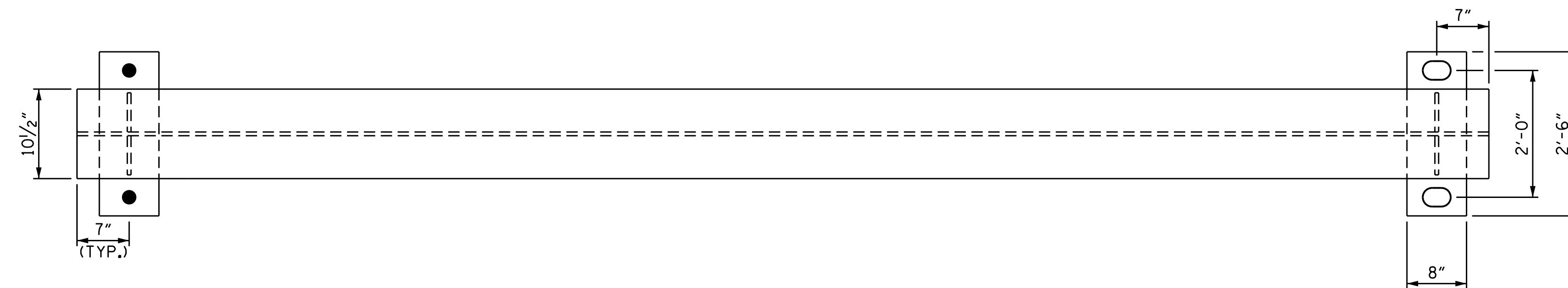
FASTENERS SHALL BE 3/4" HIGH STRENGTH IN ACCORDANCE WITH STANDARD SPECIFICATION.

EXISTING PLANS BEARING TO BEARING DISTANCE IS 44'-6" CONTRACTOR TO FIELD VERIFY DIMENSIONS.

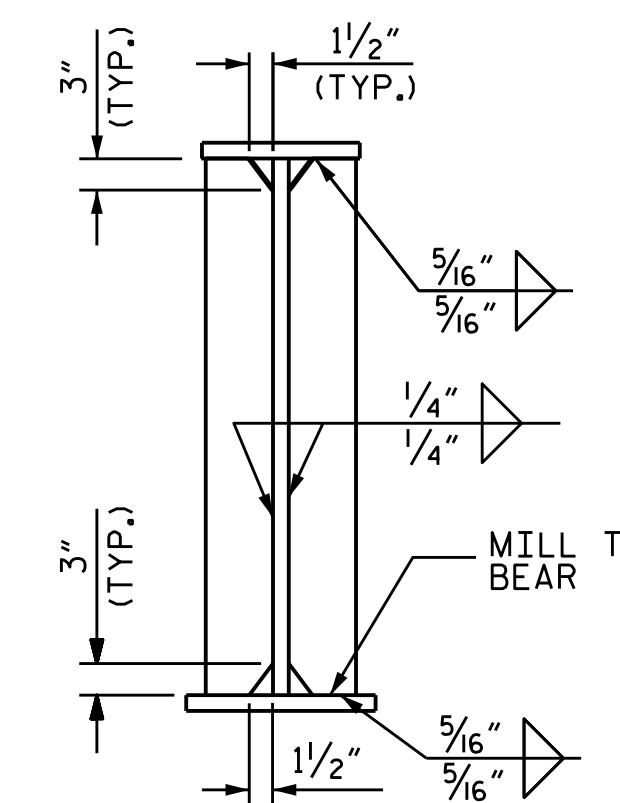
FOR PARTIAL REMOVAL OF EXISTING STRUCTURE 242, SEE SPECIAL PROVISIONS.



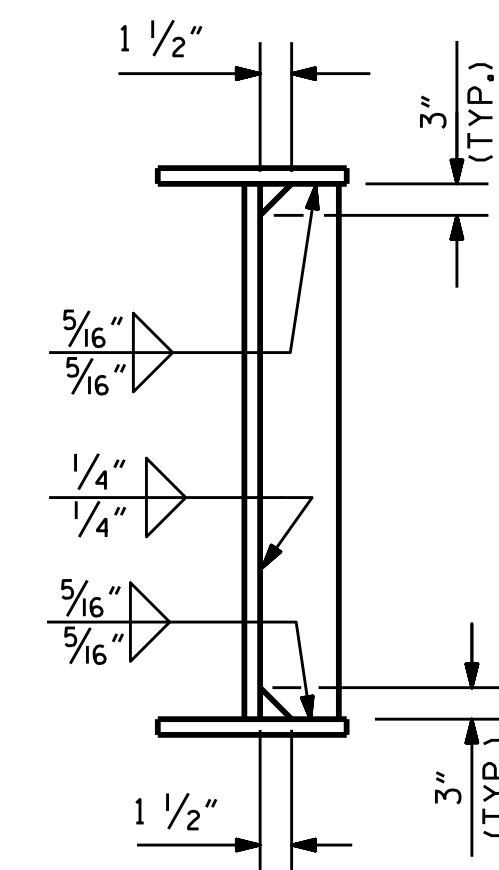
ELEVATION
(CONNECTOR PLATES NOT SHOWN FOR CLARITY.)



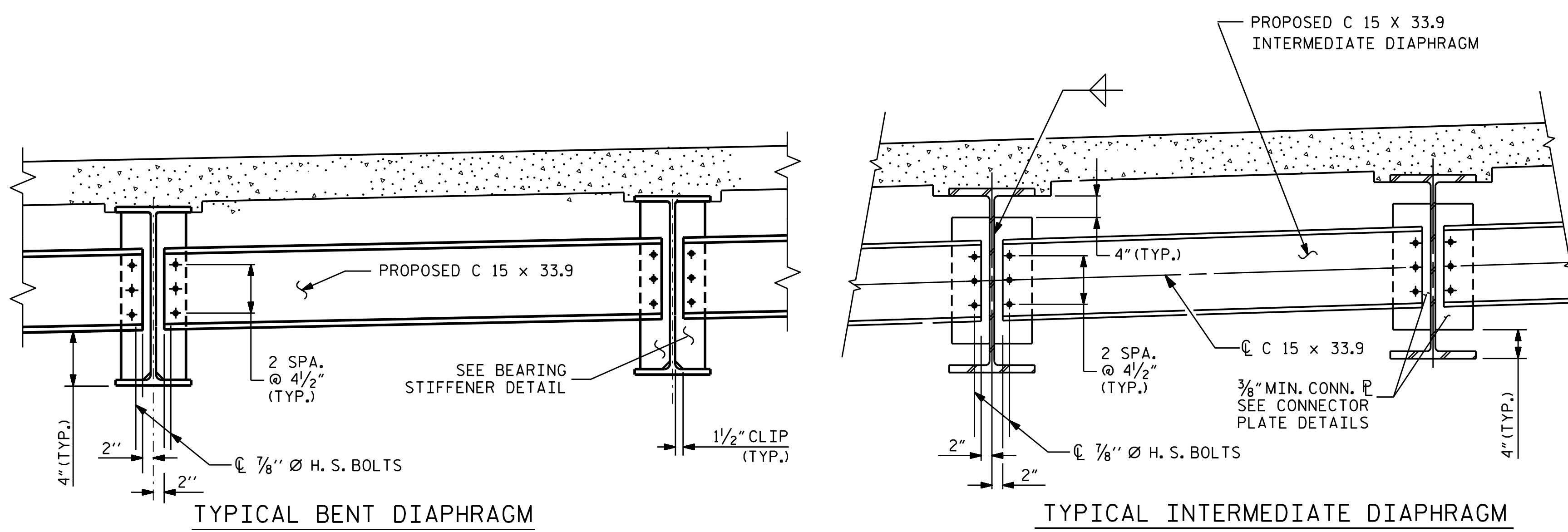
BOTTOM FLANGE



BEARING STIFFENER



INTERMEDIATE CONNECTOR P
ONLY ONE SIDE SHOWN FOR CLARITY

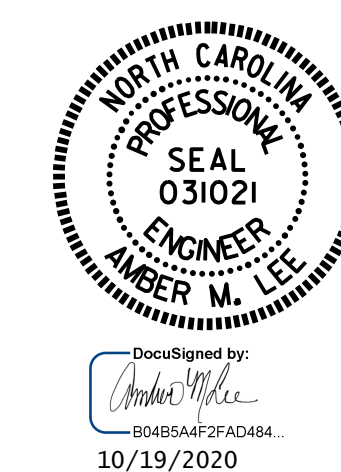


TYPICAL BENT DIAPHRAGM

TYPICAL INTERMEDIATE DIAPHRAGM

TOTAL CAMBER SHALL BE 1/16" UPWARD

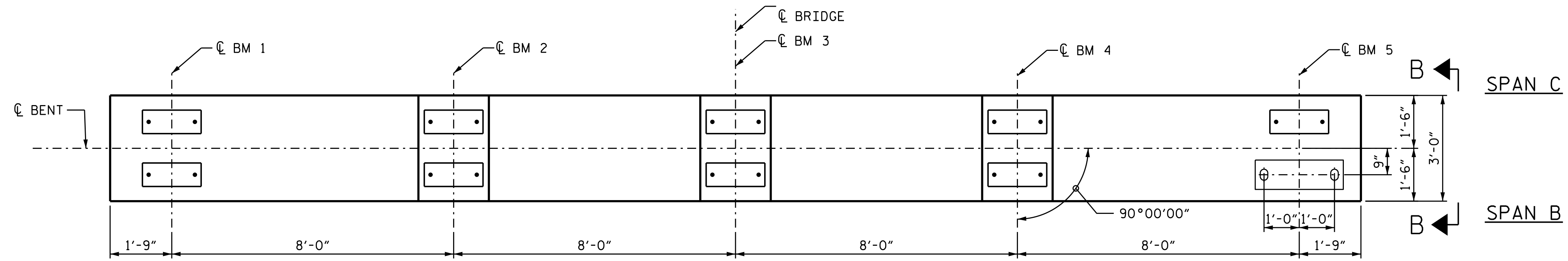
PROJECT NO. 41665.13B
BUNCOMBE COUNTY
BRIDGE NO.: 100242



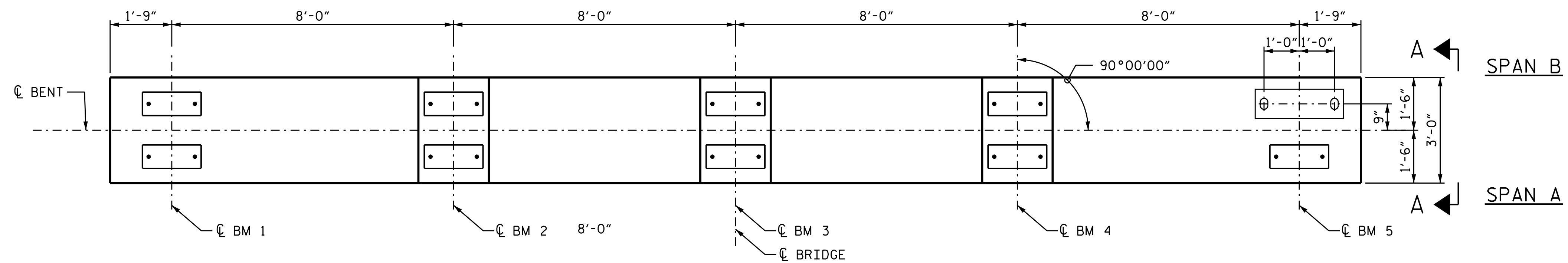
STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH					
REPLACEMENT BEAM AND DIAPHRAGMS					
SHEET NO. S-04					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
					TOTAL SHEETS 6

DRAWN BY : H.A. LOCKLEAR DATE : 05/2020
CHECKED BY : A.M. LEE DATE : 08/2020

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PLAN OF CAP FOR BENT 2



PLAN OF CAP FOR BENT 1

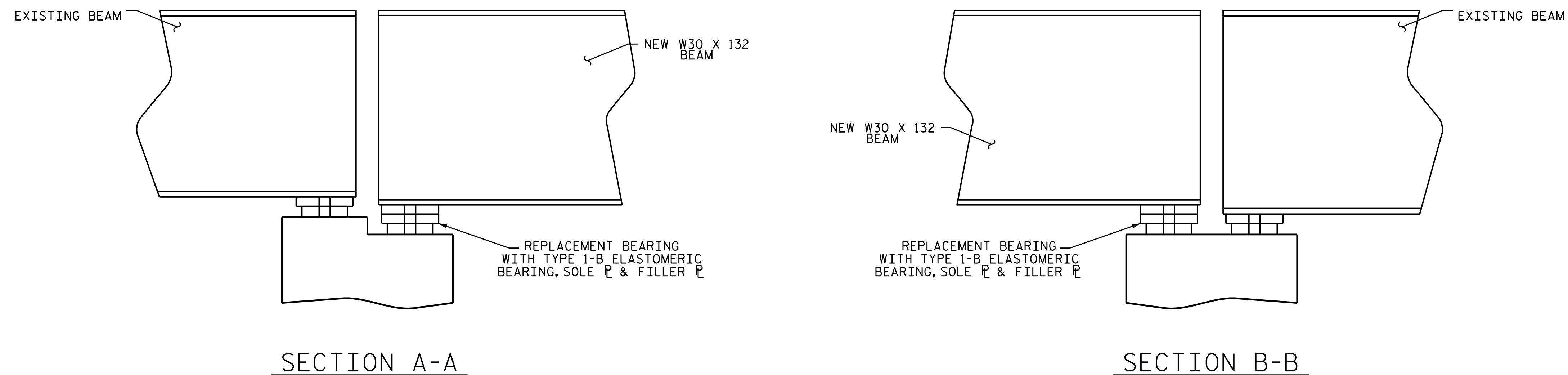
NOTES

CUT EXISTING ANCHOR BOLTS FLUSH WITH THE TOP OF THE CONCRETE CAP. CUT ENDS SHALL BE COATED WITH AN APPROVED EPOXY PAINT.

THE CONTRACTOR SHALL CORE INTO THE EXISTING BENT CAP TO INSTALL THE PROPOSED ANCHOR BOLTS. ANCHOR BOLTS SHALL BE INSTALLED USING AN ADHESIVE ANCHORING SYSTEM. THE YIELD LOAD FOR ANCHOR BOLTS IS AS FOLLOWS:
 1" DIAMETER - 14 KIPS
 1 1/4" DIAMETER - 16 KIPS
 1 3/4" DIAMETER - 18 KIPS

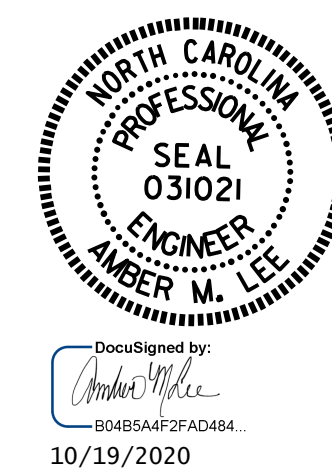
THE ANCHOR BOLT EMBEDMENT DEPTH SHALL BE 9" OR THE DEPTH RECOMMENDED BY THE ADHESIVE MANUFACTURER FOR THE GIVEN YIELD LOAD, WHICHEVER DEPTH IS GREATER.

FIELD TESTING FOR THE ADHESIVE BONDING SYSTEM IS NOT REQUIRED.



PROJECT NO. 41665.13B
BUNCOMBE COUNTY
 STATION: 100242

SHEET OF



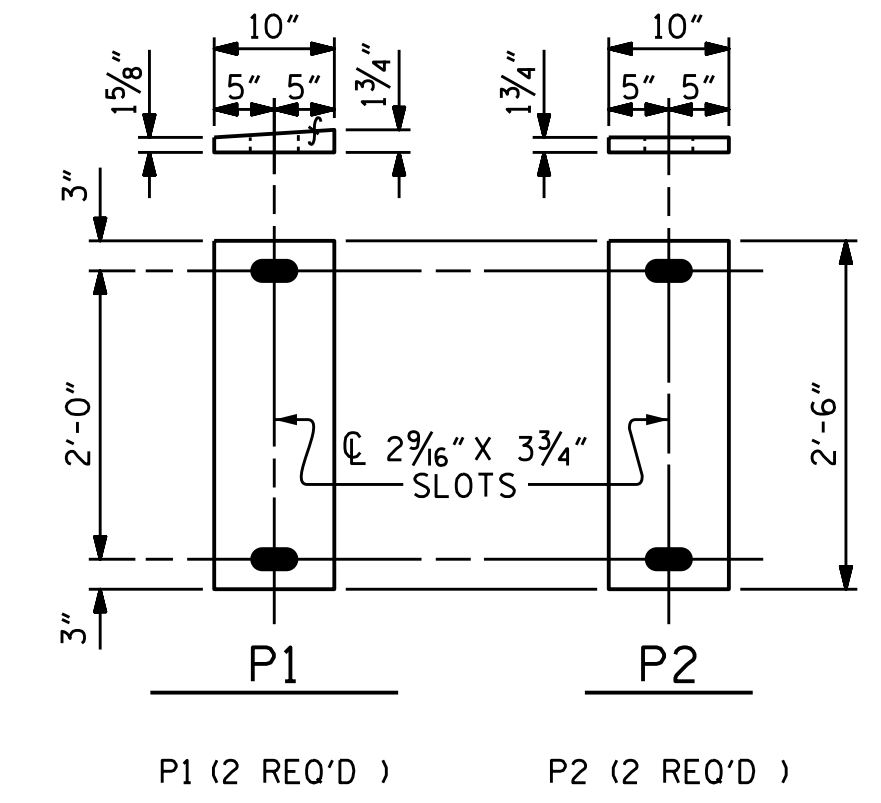
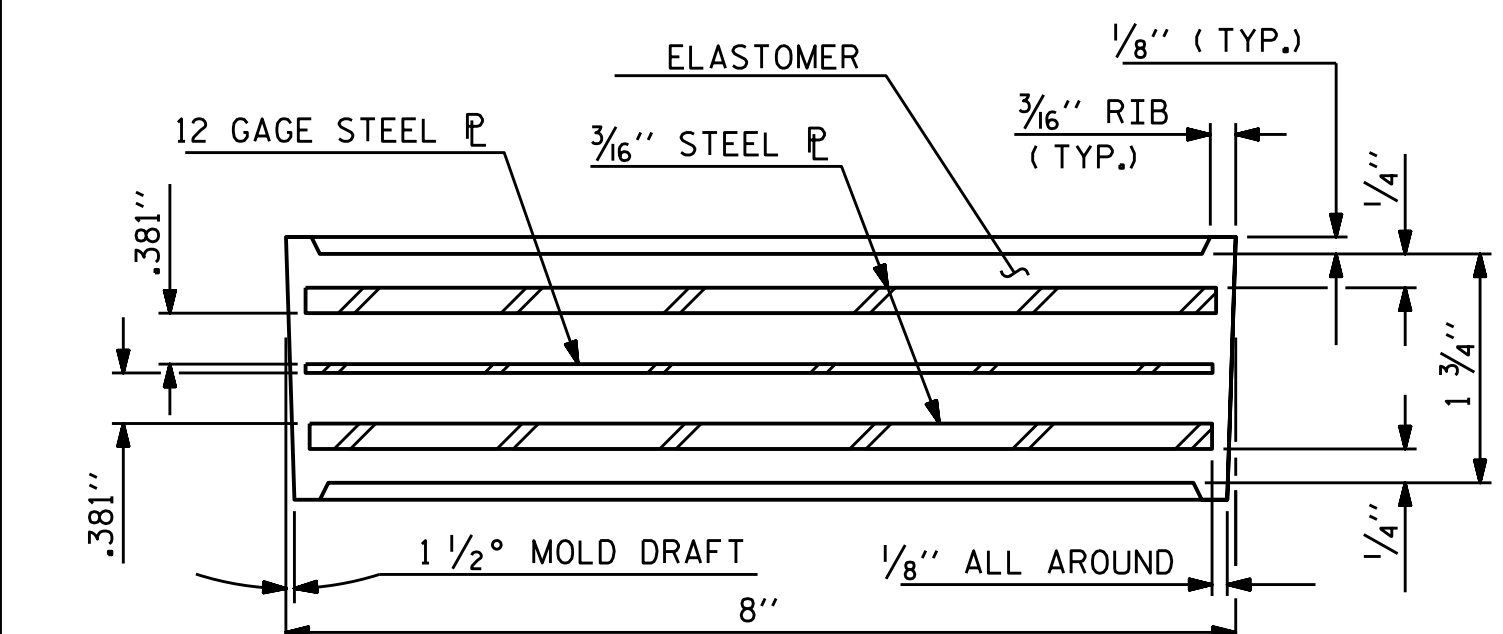
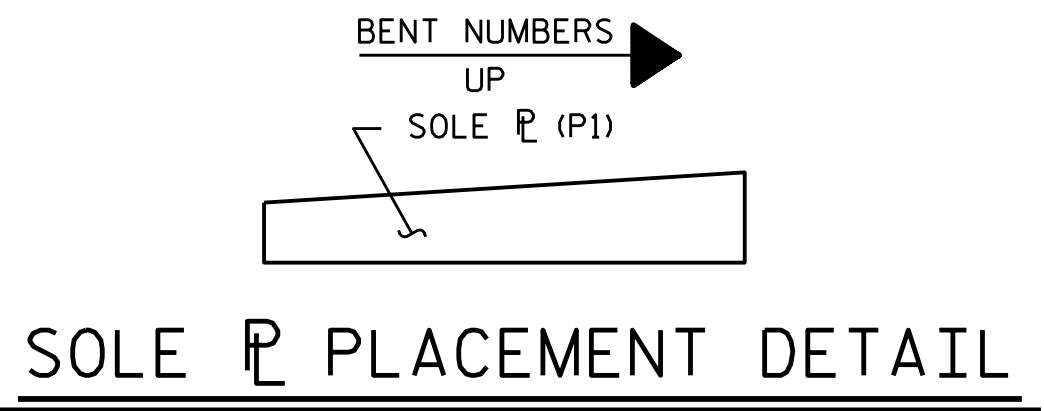
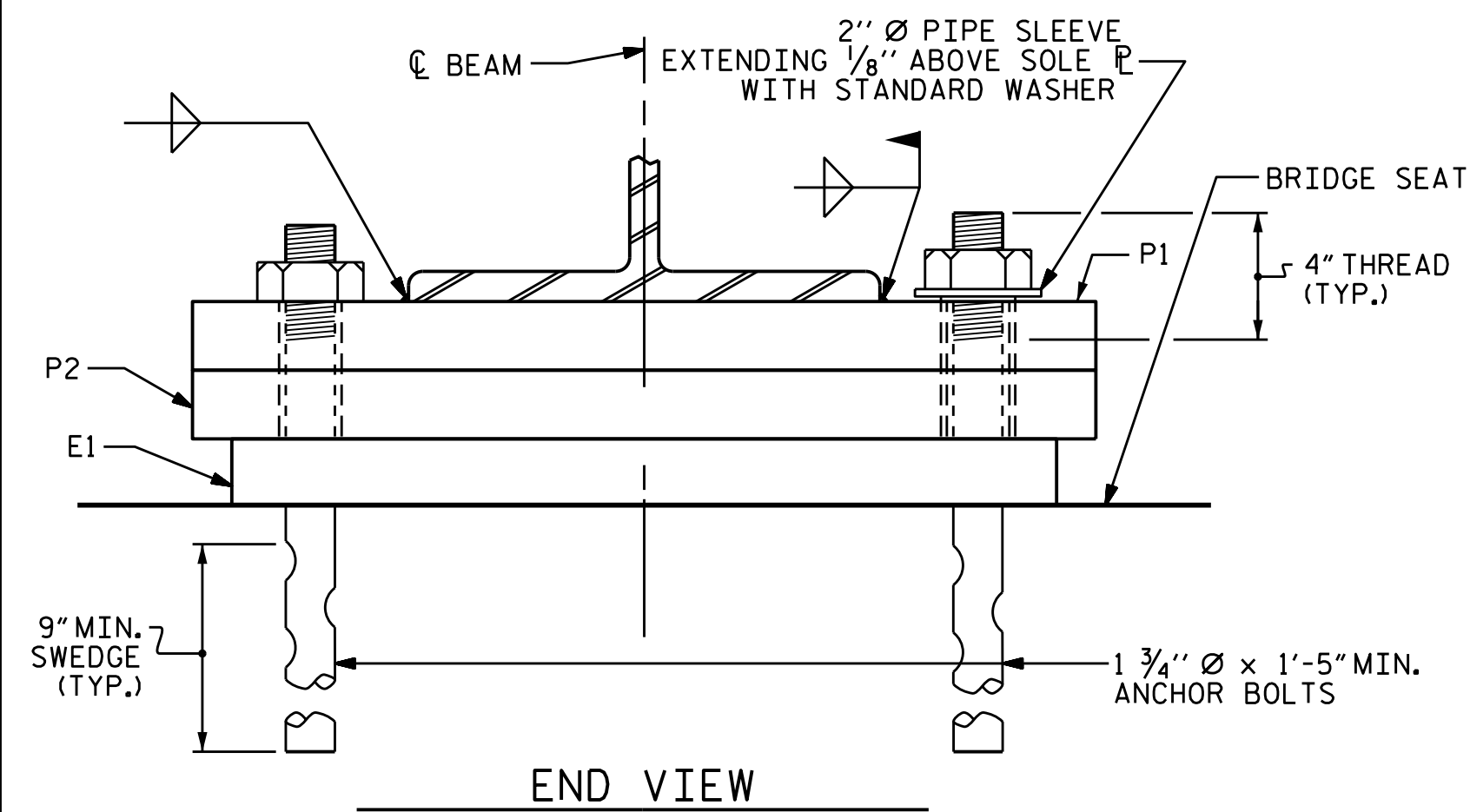
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

BEARING LAYOUT
 DETAILS

DRAWN BY : H.A. LOCKLEAR DATE : 05/2020
 CHECKED BY : A.M. LEE DATE : 08/2020

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2			4			6	



TYPICAL SECTION OF ELASTOMERIC BEARINGS

SOLE PLATE & FILLER PLATE DETAILS ("P")

NOTES

THE EXISTING BEARINGS DETERMINED FOR REPLACEMENT SHALL BE REMOVED AND REPLACED WITH BEARINGS AS SHOWN.

AT ALL POINTS OF SUPPORT, NUTS FOR ANCHOR BOLTS ARE TO BE TIGHTENED FINGER TIGHT AND THEN BACKED OFF 1 TURN. THE THREAD OF THE NUT AND BOLT SHALL THEN BE BURRED WITH A SHARP POINTED TOOL.

SOLE PLATES, ANCHOR BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

ANCHOR BOLTS SHALL MEET THE REQUIREMENTS OF ASTM A449. NUTS SHALL MEET THE REQUIREMENTS OF AASHTO M291-DH OR AASHTO M292-2H. WASHERS SHALL MEET THE REQUIREMENTS OF AASHTO M293. SHOP DRAWINGS ARE NOT REQUIRED FOR ANCHOR BOLTS, NUTS AND WASHERS. SHOP INSPECTION IS REQUIRED.

ALL WELDING SHALL BE IN ACCORDANCE WITH CURRENT APPLICABLE AWS AND NCDOT STANDARD SPECIFICATIONS.

ALL WELDS WILL BE INSPECTED AND TESTED BY THE NCDOT MATERIALS AND TEST UNIT IN ACCORDANCE WITH THE CURRENT AWS BRIDGE WELDING CODE AND STANDARD SPECIFICATIONS.

ALL SURFACES OF BEARING PLATES SHALL BE SMOOTH AND STRAIGHT.

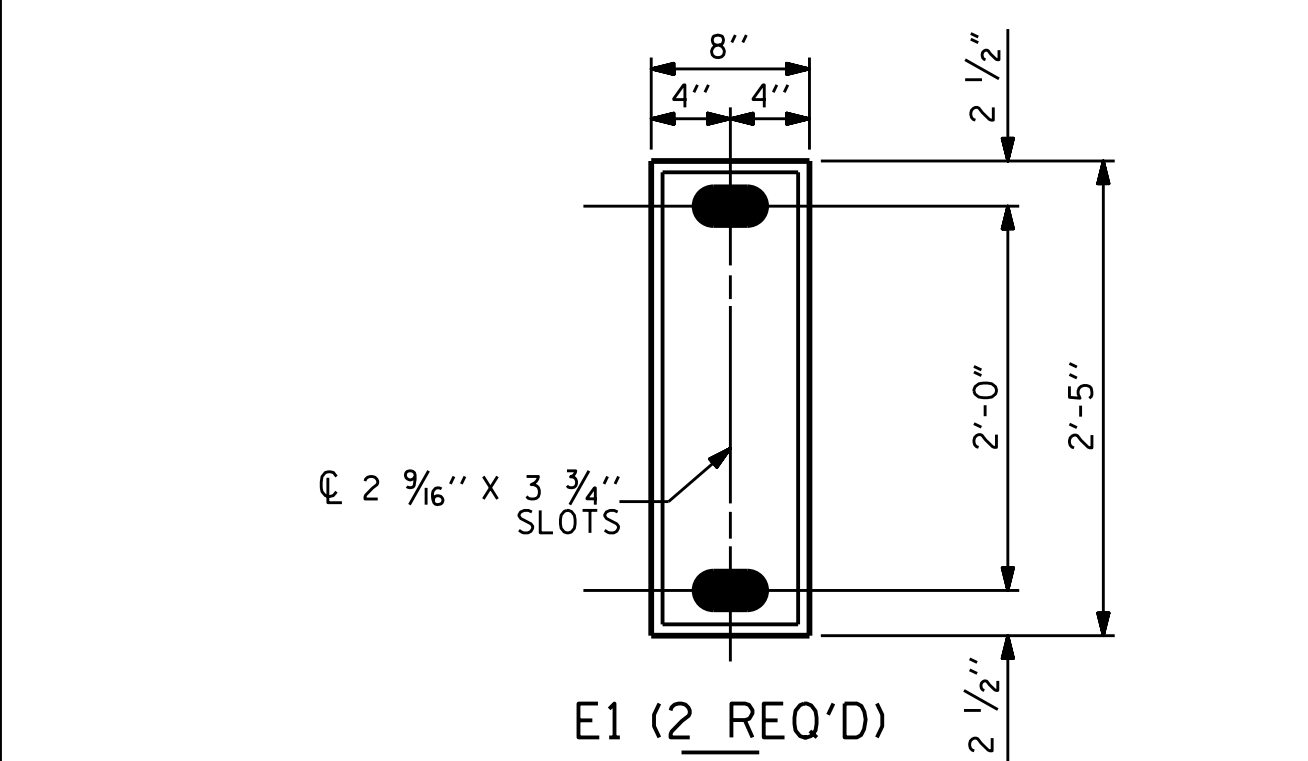
THE CONTRACTOR SHALL CORE INTO EXISTING BENT CAP TO INSTALL 1 3/4" ANCHOR BOLTS. BOLTS SHALL BE ADHESIVELY ANCHORED; SEE STANDARD SPECIFICATIONS. ADHESIVE FOR NEW ANCHOR BOLTS SHALL BE AN NCDOT-APPROVED PRODUCT.

EMBEDMENT DEPTH OF ANCHOR BOLT SHALL BE 9" OR THE DEPTH RECOMMENDED BY THE ADHESIVE MANUFACTURER TO ATTAIN PULL-OUT STRENGTH FOR THE GIVEN YIELD, WHICHEVER DEPTH IS GREATER.

FIELD TESTING IS NOT REQUIRED.

THE ELASTOMER IN THE STEEL REINFORCED BEARINGS SHALL HAVE A SHEAR MODULUS OF 0.160 KSI, IN ACCORDANCE WITH AASHTO M251.

FOR MODIFIED ELASTOMERIC BEARINGS, SEE SPECIAL PROVISIONS.

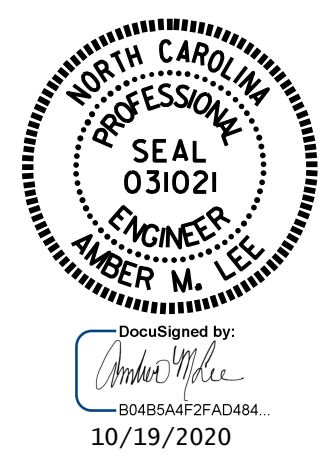


ANTICIPATED BEARING REPLACEMENT LOCATIONS

SPAN	BEAM	LOCATION
B	5	BENT 1
B	5	BENT 2

MAXIMUM ALLOWABLE SERVICE LOADS	
D.L.+L.L. (NO IMPACT)	
TYPE I-B	140 K

PROJECT NO. 41665.13B
 BUNCOMBE COUNTY
 STATION: 100242



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

BEARING REPAIR
 DETAILS
 BENT 1 & 2 BEAM 5

DRAWN BY : H.A. LOCKLEAR DATE : 05/2020
 CHECKED BY : A.M. LEE DATE : 08/2020

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

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1			3			TOTAL SHEETS
2			4			6

